

2.

Request:

Project Name: DDRO Motel / Hotel

Location:

Applicant:

Representative:

Jurisdiction:

Council District:

Case Manager:



Downtown Development Review Overlay (DDRO) District

Report to the Committee

Thru: Joseph Haberman, AICP, Urban Design Administrator
From: Kat Groskreutz, Planning & Design Coordinator
Date: June 10, 2024

Case No: 24-OVERLAY-0016
Classification: Non-Expedited
Meeting Date: June 18, 2024

GENERAL INFORMATION:

Property Address: 707 York and 726, 730, 734, 736 S 7th Streets

Applicant: Doug Schultz, Sabak Wilson & Lingo

Property Owner: Sixth & York QOZB, LLC

Description of Proposed Exterior Alteration:

New Construction: The applicant requests approval for the development of the project site (1.40 acres), which spans over 6 parcels from York to 726 S 7th Street, and projects west to Smilin' Irishman Alley between S 6th and S 7th Streets. The proposed project consists of demolition of the existing building at 734 S 7th and construction of one 2-story motel building facing the streets. A 2-story hotel building facing the west alley will be developed as part of a subsequent phase. The total proposed square footage between the two buildings will be approximately 49,500 square feet with a maximum height of 25' at the southwest corner of the motel structure.

The motel is an L-shaped structure with its two main facades facing S 7th and York Streets. It has articulated façades and parapet lines. Butterfly roof designs are used at the north and southwest corners of the building. The main entrance is located to the northeast end of the building, defined by a span of wrap around storefront glass and full-lite double person doors. The materials are a mix of brick veneer, horizontal metal siding and stucco, with accents of wood look cement board. Large double windows are located continuously along both floors. An area for public art is proposed for the southeast corner facing York Street.

The facades internal to the site contain a second entrance to the lobby from the parking lot with a span of storefront glass and full-lite double person doors. The ground floor contains a wall that semi-encloses the walkway to the room entrances and has

perforations for viewing and pedestrian access from the parking lot. Stairs are located on either end and toward the middle of the L which lead to an open second floor balcony walkway providing access to the upper floor rooms. Each room entrance is accented by the wood look cement board.

The internal parking lot will have access from the north end of S 7th and west from Smilin' Irishman Alley. It will contain 70 parking spaces. The dumpster enclosure will have brick veneer and solid metal gates and be accessed on the west side of the site by the west alley entrance. An open space is proposed at the northwest corner and will be partially landscaped.

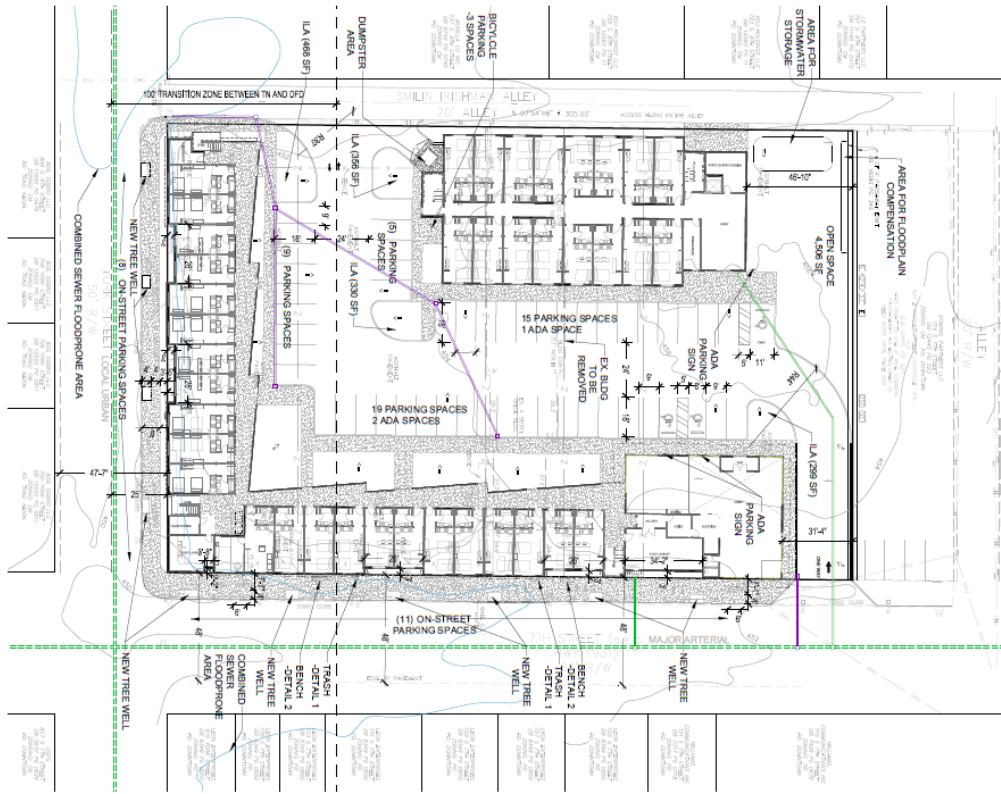
The hotel building is part of a later phase for the site, and the details of its design are yet to be determined. While staff acknowledges this additional building in this application and is conceptually reviewing its general footprint and form, its final design and materials must be reviewed and approved under a subsequent overlay permit.

Streetscape Improvements: The proposed streetscape plan includes improvements such as 10 street trees, landscaped areas in front of the building insets, metal benches, and metal trash receptacles. Two drive entries are utilized for vehicular access along 7th Street and Smilin' Irishman Alley and provide pedestrian access into the site. Lighting details have not been provided at the time of this report.

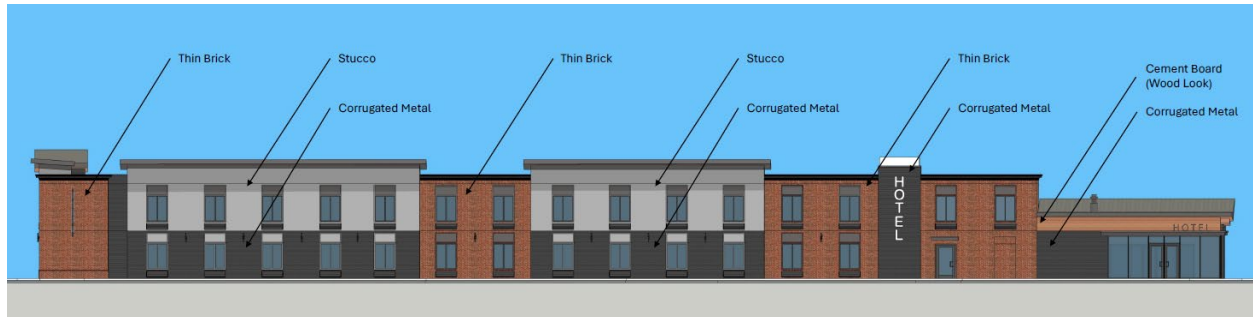
Demolition: To construct the new project, an existing, circa 1880, four-story building located at 734 S 7th, central to the subject site, is proposed to be demolished. Per Savannah Darr, Metro Historic Preservation Officer, this building is not eligible for listing on the National Register of Historic Places. She finds:

The building has lost its historic integrity in design, workmanship, and materials as the building has been significantly altered. What began as a residential building was converted to mixed use around the 1920s. It was then further altered in the 1950s and then 1970s by the former J.W. Russell Electric Co. Window openings have been heavily modified, and all decorative ornamentation has been removed. Furthermore, the building has lost integrity in association, setting, and feeling as the once intact block is now almost completely vacant. There is no surrounding historic context remaining. Thus, the late 19th century building is not eligible for listing in the National Register of Historic Places. It does not meet the threshold requirements of LMCO 150.110.

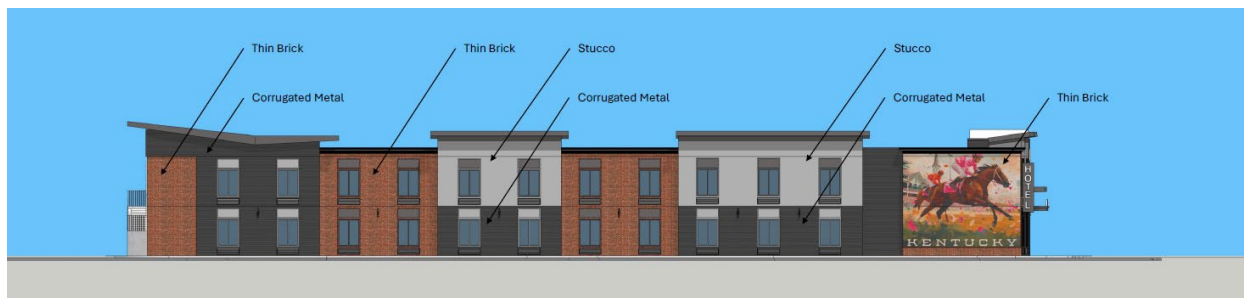
The applicant asserts that due to its design, condition, and location, the building cannot be retained or practically incorporated into the modern project.



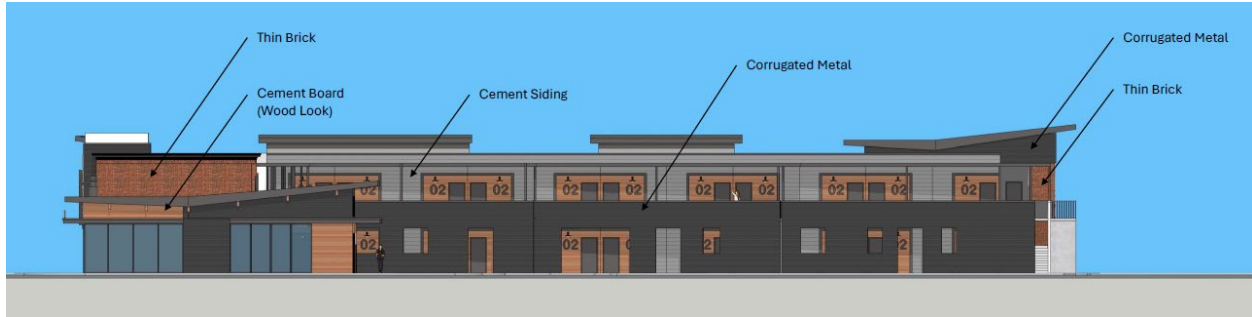
Site Plan



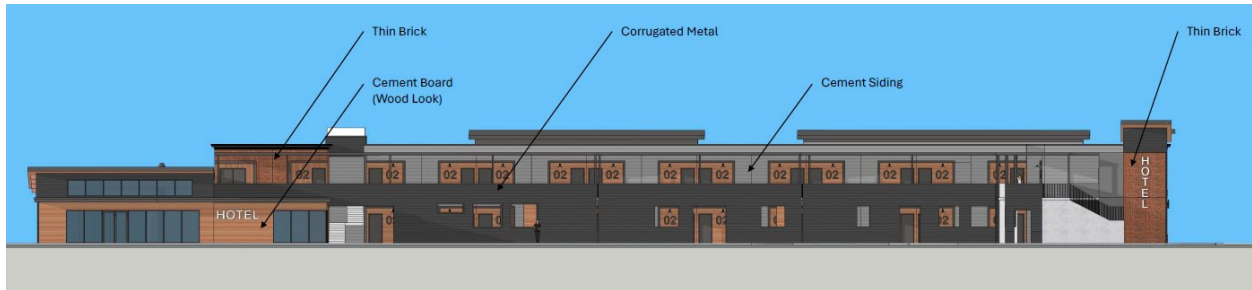
S 7th (East) Façade



York (South) Façade



Internal Parking Lot (North) Façade



Internal Parking Lot (West) Façade



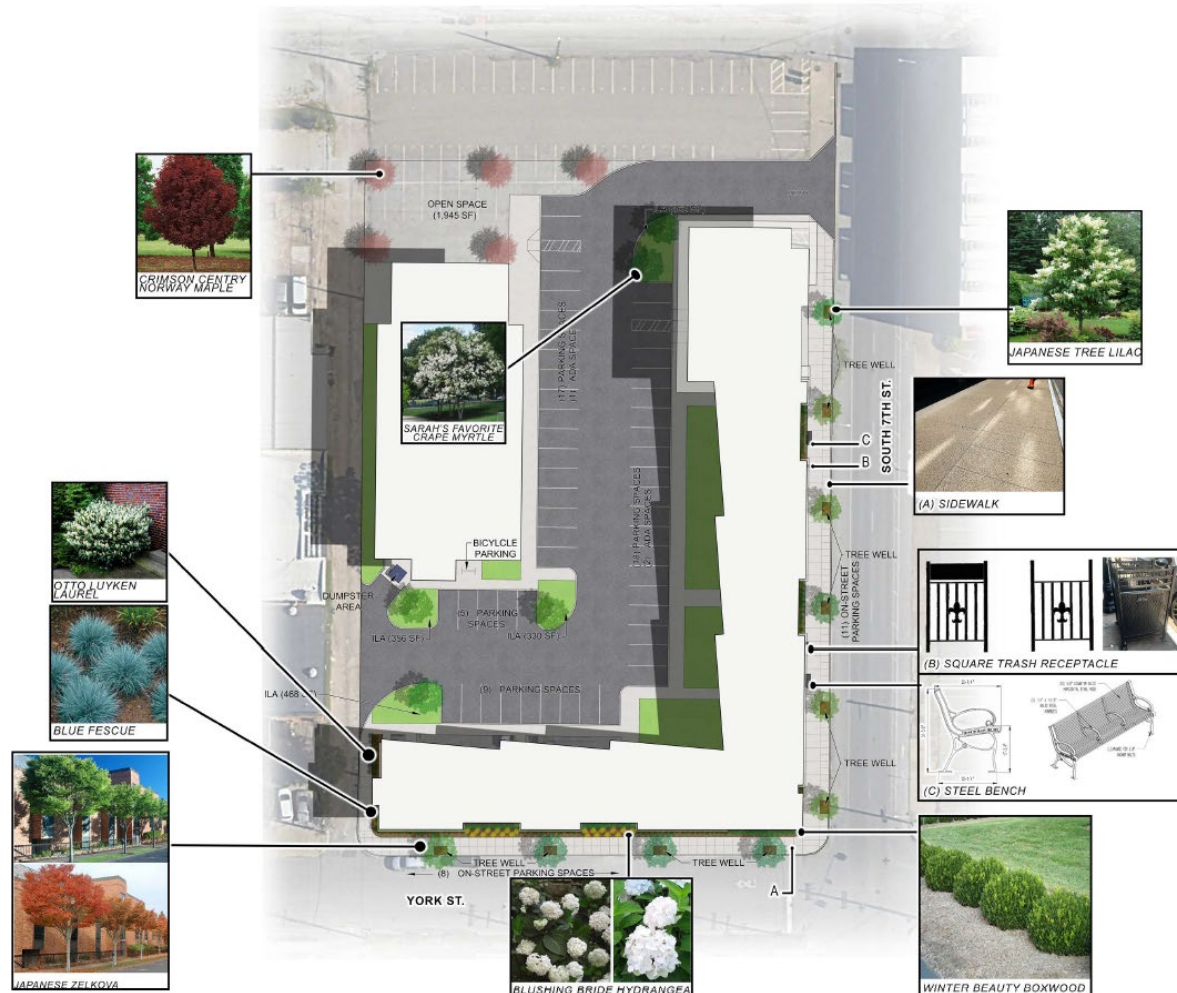
Northeast Corner Entry and East Façade



Northeast Corner



Southeast Corner



Landscape/Streetscape Plan

Communications with Applicant, Completion of Application:

The applicant discussed the project and design details with staff prior to submitting the Overlay Permit application on May 6, 2024. The application was classified as requiring a non-expedited review by the Urban Design Administrator on the same day. The application will be reviewed by the DDRO Committee on June 18, 2024.

As part of this proposal, applications for a Category 3 development plan (24-CAT3-0001), land development code waiver (24-WAIVER-0011), and land development code variance (24-VARIANCE-0008) for 2-story building in the Downtown Form District have been submitted. A waiver for parking location may also be required.

FINDINGS

Guidelines

The following DDRO Principles and Design Guidelines are applicable to the proposal: **1 – Site Planning, 2 – Building Massing, 3 – Building to Context, 4 – Building to Pedestrian, 5 – Parking, Vehicular Use and Access, 6 – Open Space, 7 – Street and Sidewalk Character, and 9 – Public Art.** Staff's findings of fact and conclusions with respect to the Guidelines are attached to this report.

Site Context

The subject property is six parcels bordered by 714 S 7th Street to the north, S 7th Street to the east, York Street to the south, and Smilin' Irishman Alley to the west. The parcel is zoned CM and located in the Downtown form district. The only remaining structure on the site is a historic, circa 1880, 4-story masonry commercial building. The rest of the parcels are surface parking lots. The subject property is located at the far southern edge of the DDRO district, south of W Broadway, and surrounded by a mix of 1 to 2-story commercial buildings and surface parking lots.

Existing Conditions of Site



S 7th – Facing Southwest, Google 2022



Remaining Building at 734 S – Facing West, Google 2022



Corner of York and S 7th – Facing Northwest, Google 2022



Corner of York and Smilin' Irishman Alley – Facing Northeast, Google 2022



Smilin' Irishman Alley – Facing Southeast, Google 2019

Surrounding Context



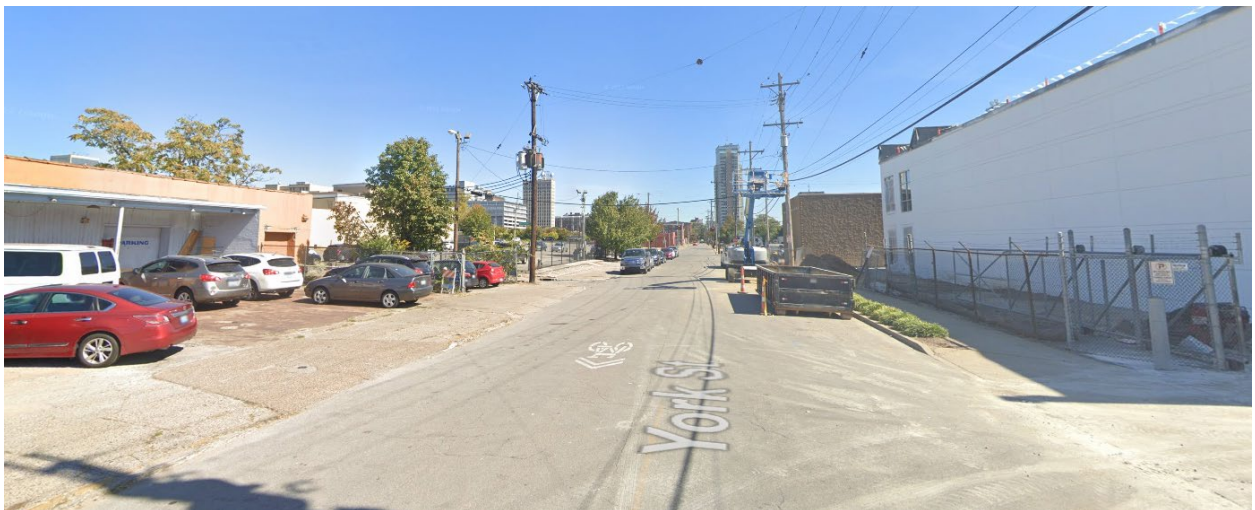
S 7th – Facing South, Google 2022



S 7th – Facing North, Google 2022



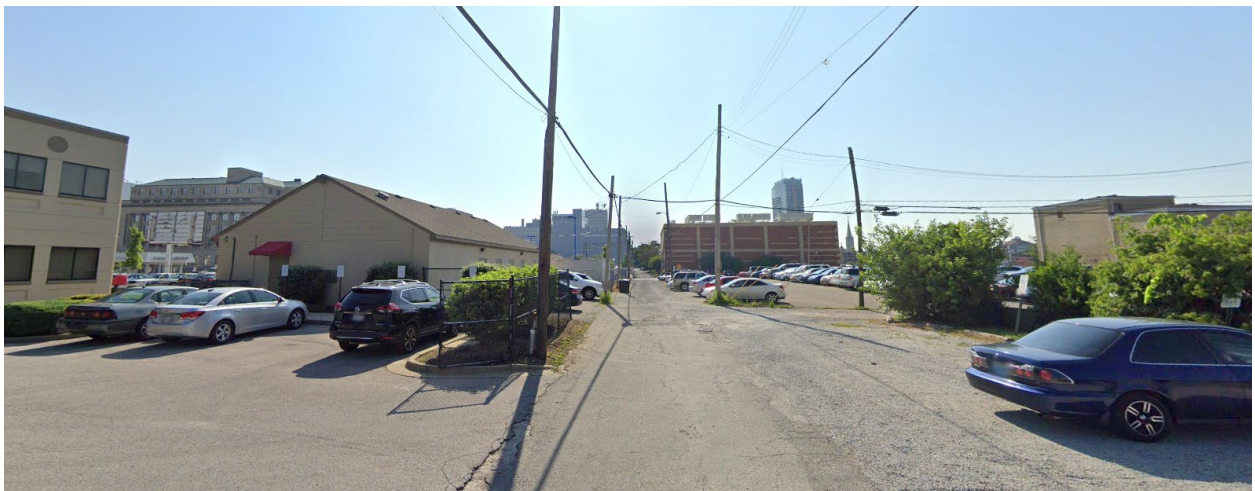
York – Facing West, Google 2022



York – Facing East, Google 2022



Smilin' Irishman Alley – Facing North, Google 2019



North Alley – Facing East, Google 2019



North Alley – Facing West, Google 2019

CONCLUSIONS

Demolition

To construct the new project, the remaining commercial building at 734 S 7th Street is to be demolished. Per Savannah Darr, Metro Historic Preservation Officer, this building has been found not eligible for listing on the National Register of Historic Places and therefore is not a Contributing Historic Structure in terms of the District's Guidelines.

The Urban Design Administrator and Historic Preservation Officer conclude that the building's location in the middle of the subject site, and being setback from the street, make it particularly difficult to incorporate into a larger scale development. Its existing setback also does not meet the current design guidelines which call for building facades built to the street. The retention of the building may have negative economic impacts to the project and affect its overall feasibility.

New Construction

The new motel building will include 48 rooms, 24 on each floor. It is contemporary in style and has architectural details that are common of other modern motel/hotel projects. The materials of brick, horizontal metal siding, stucco, and wood look cement board are compatible with the surroundings on the southern fringe of the district. The design places some emphasis on the corners, having these portions of the building serve as focal points through height changes, unique roof shapes, public art, or wrap around storefront glass to define the main entrance. However, there is only one main entrance and one secondary entrance facing a street, both located to the north of the motel building facing S 7th. Staff highly recommends at least one more door and awning, similar in style and design to the secondary door proposed on S 7th, be added along York Street to access the southeastern stairwell. It is not required to be a public access door or unlocked at all hours, but could be an emergency exit or key card access door. The presence of a door/entry in this location would make the overall design more compatible with the guidelines.

While a 2-story height is generally discouraged within the DDRO and Downtown Form District, this site is located at the southern fringe of the DDRO, ½ block south of W Broadway, in an inconsistently developed area with a mix of building types and forms. All parcels surrounding the subject site are either smaller commercial buildings that are 1 to 2-stories in height, or surface parking lots. Given this context, 2-stories is an appropriate height for the site.

The parapet line is articulated, and in conjunction with the articulated building façade and changes in material finishes, the longer facades are broken up to increase interest at the pedestrian level. The pedestrian experience is enhanced by the building's ground level storefront, windows, and proposed public art. Additional elements that promote a pedestrian-friendly environment include the building's general location along the property line, insets with landscaping along the sidewalk, and street furnishings of benches and trash receptacles. The addition of street trees and other landscaping will help with the

pedestrian experience and assist to break up the massing of the building from pedestrian and vehicular perspectives. These elements and the internal open space will also improve the site's sustainability in terms of its impact on urban heat and stormwater, as it is currently almost 100% permeable surfaces.

Staff recommends the open space be developed in a way to provide either public access or amenity space for the hotel/motel guests. Details like fencing, patio or walkways, public art, or additional landscaping is encouraged in this area. Staff also request information regarding the interim treatment to the area set aside for the later phase hotel building.

The surface parking lot is positioned internal to the site, behind the motel building and almost completely screened from York and S 7th Streets. The current six curb cuts along these streets will be reduced to one along S 7th, with additional access from Smilin' Irishman Alley to the west. This will greatly reduce conflicts between pedestrians and cars along the subject site.

Overall, the proposed project will bring increased activity to an edge of the District that has minimal development. The site is largely vacant and the addition of the two buildings will more positively contribute to urban density in the area. The motel will bring a continuous street wall along two mostly bare sections of S 7th and York, remove multiple curb cuts, increase street trees and landscaping, and add greater vibrancy to this area and the DDRO.

RECOMMENDATION

The proposed demolition and new construction generally comply with the applicable Design Guidelines for the DDRO District and meets the overall intent of the District.

Considering the information furnished, the Urban Design Administrator recommends the application for an Overlay Permit be **approved with the following conditions**:

- 1. All glass on the ground level of the S 7th and York Street facing facades shall be clear. Front glass shall not be tinted, smoked, or have added reflection or mirroring. If the glass is low-E, it shall be clear.**
- 2. The applicant shall consider the installation of a glass person door with awning on the south York Street façade accessing the southeast stairwell. If included, the applicant shall submit an updated elevation to Planning staff for review and approval.**
- 3. Rooftops shall not look cluttered from any pedestrian vantage point. All mechanical or utility equipment shall be well-integrated into the overall design.**
- 4. Exterior lighting details shall be provided to staff for review and approval prior to construction taking place.**

5. Details regarding the design of the open space at the northwest corner of the subject site, including but not limited to potential fencing/gates, furnishings, patios or walkways, lighting, or public art, shall be provided to staff for review and approval prior to construction.
6. The internal 2-story hotel building shown at the west of the site is only conceptually approved. When design details are finalized for the hotel building phase, the applicant shall submit documentation to Planning staff to be reviewed under a subsequent Overlay Permit.
7. The applicant shall submit plans for the interim treatment of the proposed hotel footprint area to staff for review and approval. Staff recommends that this area be landscaped or otherwise pervious in nature in the interim.
8. Any public art is not considered under this application and shall be submitted to Planning staff to be reviewed under a subsequent Overlay Permit.
9. Any future signage is not considered under this application and shall be submitted to Planning staff to be reviewed under a subsequent sign and Overlay Permit.
10. Prior to the issuance of a building permit for the new motel building, finalized design details and materials shall be submitted to staff for final review and approval. After a building permit has been issued, if the design or materials change, the applicant shall contact staff for review and approval prior to installation.

1 Site Planning

Checklist

Each downtown site lies within a specific neighborhood, adjacent to specific traffic corridors and intersections, and may be adjacent to areas of different intensity of development; may be near public open spaces; may be near historic and/or significant structures; and may contain historic and/or significant structures. These basic issues should be evaluated for the project site and considered at the earliest stages of concept development.

+	Meets Guidelines	NA	Not applicable
-	Does not meet Guidelines	TBD	To be determined; insufficient Information
+/-	Meets Guidelines with conditions as noted		

Guideline	Finding	Comment
SP1 <i>Building placement and orientation.</i> It is intended that downtown develop as an urban environment with a consistent, animated street wall which defines a physical area that is friendly, active, and safe for the public. Generally speaking, new construction should build to the front property line and extend the width of the property. Corner properties should be built to both frontages. Properties with three or more frontages should give consideration to the relative character of the frontages and focus development accordingly - primary consideration should be given to orientation toward major thoroughfares.	+	The motel building is built to the frontages of York and S 7 th , with animated street walls. It extends the entire width of York and almost the full width on S 7 th , with the exception of a vehicular access drive to the north.
SP2 <i>Public space.</i> Setbacks from the property line may be considered provided the setback area is developed as a public open space and amenity or as a location for exterior activity related to ground floor usage of the buildings, such as outdoor dining or retail. Consideration should be given for providing public open space on sites that align with other significant urban elements such as open spaces or vistas, significant neighboring structures or public institutions, axis or terminus of the street grid	NA	Public open space is not provided. There are minimal setbacks from the street to provide articulated wall facades and areas for landscaping.
SP3 <i>Preservation of existing structures.</i> Existing structures that are identified locally or nationally as having significant historic character should be retained and incorporated into new development. Modifications to these structures shall be in accordance with the latest edition of the Secretary of the Interior's Standards for Rehabilitation. No application to demolish these structures shall be approved unless the applicant is able to demonstrate that: a) rehabilitation of the structure or its replacement will have a greater positive impact on the economic vitality of the District than preserving the existing structure and that the construction of the new structure would not be possible or economically feasible without the demolition of the existing structure; or b) that the applicant cannot obtain a reasonable economic return from the property or structure unless the existing structure is demolished. Development within the West Main Street local preservation district shall be reviewed by the Historic Landmarks and Preservation Districts Commission.	+	An existing historic building is to be demolished. Per HPO Savannah Darr, it is not eligible for the National Register of Historic Places and does not have significant historic character.
SP4 <i>Site access.</i> Careful consideration should be given to vehicular site access, on-site circulation, parking, and sufficient access for storage and collection of waste and recycled materials to minimize impacts to the street wall, pedestrian environment, and the streetscape. Consideration should also be given for other types of access such as pedestrian, public transit, and bicycle.	+	Vehicular access has been limited to one location along S. 7 th at the north of the subject site, and one location from Smilin' Irishman Alley at the southwest. Parking is internal to the site and will be mostly screened by the motel building. The dumpster enclosure will be accessed from the access point along Smilin' Irishman Alley.

2 Building Massing

Checklist

Develop an architectural concept and compose the major building elements and massing to reinforce desirable urban features in the surrounding area and district. Compose the massing of the building to create transition to the height, bulk, and scale of development in nearby less intensive zones.

+ Meets Guidelines

NA Not applicable

- Does not meet Guidelines

TBD To be determined; insufficient Information

+/- Meets Guidelines with conditions as noted

Guideline	Finding	Comment
BM1 Arrange the mass of the building in response to the following as applicable: 1) Distinct and noteworthy characteristics of the district/neighborhood; 2) Adjacent landmark or noteworthy building; 3) Major public entity or institution nearby; 4) Neighboring buildings that have employed distinctive and effective massing compositions; 5) Public views and vistas; 6) Potentially negative micro-climate issues such as extensive shadows and urban wind effect.	+	The subject site is surrounded by 1 to 2-story commercial buildings of limited architectural note and surface parking lots. It will have similar height and massing to its surroundings, and will not negatively impact views, vistas, or create climate issues. The site is currently almost 100% impervious. Landscape and open space are proposed to help offset heat island effects that may currently exist.
BM2 Compose the massing of the building to relate strongly to nearby buildings and create a transition to the height, bulk, and scale of development in nearby, less intensive zones. Buildings on zone edges should be developed in a manner that creates a step in perceived height, bulk, and scale between the development potential of the adjacent zones. Factors to consider in analyzing potential height, bulk, and scale impacts include: 1) Distance from a less intensive district edge; 2) Differences in development standards between abutting neighborhoods; 3) Type and amount of separation between districts, such as property line, alley or, street.	+	The subject site is on the southern edge of the district and surrounded by 1 to 2-story commercial buildings and surface parking lots. It will have similar height and massing to its surroundings and creates a transition from the greater heights located north along Broadway and further in the district.
BM3 Design a well-proportioned and unified building. Compose the massing and organize the interior and exterior spaces to create a well-proportioned building that exhibits a coherent architectural concept. Design the architectural elements and finish details to create a unified building, so that all components appear integral to the whole.	+	The motel is well-proportioned for the site and location, and has a unified design with a coherent, modern architectural concept. The finishing details are carried throughout and used to define sections of the building while making them integral to the whole. The hotel building design is not being reviewed under this application. See conditions of approval.
BM4 The building composition should include a well-defined base at the pedestrian level that fits well into its context. As a general minimum the base should be two to three stories.	+	The motel has a well-defined base though the use of differing materials, with brick and darker metal siding grounding the building at the pedestrian level. While it is only one story, the whole building is only two, so is appropriate in this context. The hotel building design is not being reviewed under this application. See conditions of approval.
BM5 To allow adequate light and air to reach the street level, high rise buildings (over 14 stories) should generally: 1) Be located about 100 feet from other high rise buildings within the same block; 2) Have upper stories which are progressively narrower; the higher the story, the narrower.	NA	
BM6 Rooftops should not look cluttered from any pedestrian vantage point. All mechanical or utility equipment should be well-integrated into the overall design.	+/-	Mechanical and utility equipment details have not been provided at this time of this report. See conditions of approval.

3 Building to Context

Checklist

A certain amount of architectural diversity is expected in any downtown. However, buildings should be “good neighbors” by relating well to the common patterns of windows, entrances, cornice lines and column spacings around them and reinforcing the overall character of their immediate surroundings. Develop an architectural concept and compose the major building elements to reinforce desirable urban features in the surrounding context and district.

- + Meets Guidelines
- Does not meet Guidelines
- +/- Meets Guidelines with conditions as noted
- NA Not applicable
- TBD To be determined; insufficient Information

Guideline	Finding	Comment
BC1 Be compatible with the general character of nearby buildings in terms of facade organization, materials, finishes, scale of detail, and respecting established horizontal and vertical elements and spacing in the nearby context such as cornice lines and pier/column spacing.		The motel design is compatible with the general character of surrounding buildings, which are 1 to 2-story commercial buildings of limited architectural note. The façade organization, materials, finishes, scale of detail, and vertical/horizontal elements are appropriate for the site, its surroundings, and the southern edge of the district.
	+	The hotel building design is not being reviewed under this application. See conditions of approval.
BC2 Reinforce the character of nearby buildings having historic or architectural significance by developing designs that respect established cornice lines, horizontal and vertical facade organization, and massing of historic buildings in the context.		Most of the nearby buildings are modern infill or have limited historic or architectural significance. The motel has defined cornice lines and façade organization and massing that fits the surrounding context.
	NA	The hotel building design is not being reviewed under this application. See conditions of approval.
BC3 Follow the rehabilitation standards in the latest edition of the Secretary of the Interior's Standards for Rehabilitation whenever historic or architecturally significant structures are to be altered, expanded, or when new construction is to occur adjacent to such structures.		The building to be demolished was found by the HPO to not be eligible for the National Register of Historic Places or architecturally significant.
	NA	

Reference: SOI Standards for Rehabilitation

- (a) A property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces, and spatial relationships.
- (b) The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided.
- (c) Each property will be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken.
- (d) Changes to a property that have acquired historic significance in their own right will be retained and preserved.
- (e) Distinctive material, features, finishes, and construction techniques or examples of craftsmanship that characterize a property will be preserved.
- (f) Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary or physical evidence.
- (g) Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.
- (h) Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.
- (i) New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic material, features, size, scale, and proportion, and massing to protect the integrity of the property and its environment.
- (j) New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environments would be unimpaired.

4 Building to Pedestrian

Checklist

People should have strong visual connections to buildings as a strong building-to-pedestrian relationship helps make downtown feel more inviting and active 24 hours a day. Therefore develop the street level of the building's exterior to create safe, inviting, and active environments and spaces to engage pedestrians. These environments are defined by the uses that occur within them, physical space for them to occur, and articulation of the physical surroundings.

+ Meets Guidelines

NA Not applicable

- Does not meet Guidelines

TBD To be determined; insufficient Information

+/- Meets Guidelines with conditions as noted

Guideline	Finding	Comment
BP1 Where sidewalk width is limited, consider setting portions of the building back slightly to create spaces conducive to pedestrian-oriented activities.	+	There are articulations along the two street facing facades of the motel to allow space for landscaping and benches in the ROW. The hotel building design is not being reviewed under this application. See conditions of approval.
BP2 Articulate the building facade to provide an engaging pedestrian experience with design elements such as open shop-fronts or arcades, multiple entries, merchandising and display windows, street front open space with artwork or furniture, awnings, signage, and light fixtures. In some instances raised landscaped beds may be appropriate. As a general rule 50% of the wall surface at the sidewalk level should be transparent, utilizing glazing that is not highly tinted or reflective.	+	The motel building façades are articulated. The hotel building design is not being reviewed under this application. See conditions of approval.
BP3 Utilize building materials characteristic of the area having texture, color, pattern, and a higher quality of detailing.	+	The motel materials and detailing vary and are appropriate for this area within the district. The hotel building design is not being reviewed under this application. See conditions of approval.
BP4 Variations on the facade plane such as inset entries, building piers, and other details can assist in providing relief to long expanses of building wall.	+	While there are not particularly long expanses of wall, the design includes acceptable variations to provide relief. The hotel building design is not being reviewed under this application. See conditions of approval.
BP5 Building entries should be clearly identifiable and visible from the street. Principle building entrances should face the street. Entrances should be inviting and easily accessible. They should have a high level of articulation and be well-lit. Canopies or awnings provide protection from the weather.	+/-	One clearly identified main entry with wrap around storefront glass and full-lite double person doors into the lobby is located at the far north of the motel building facing S 7 th . A secondary, single person door with awning is located slightly south of the main entry. Another main entry to the lobby is internal to the site and does not face a street. There are no entries facing the corner or York. Lighting details were not provided by the time of this report. See conditions of approval. The hotel building design is not being reviewed under this application and will be internal to the site. See conditions of approval.

BP6	Changes in sidewalk material aid in defining exterior spaces and entryways.		No changes in the sidewalk material are proposed on the site plan. This delineation is not required along all entryways; however ideally access to sites in the district occurs along alleyways when available. Staff suggests utilizing a different material or adding striping in the sidewalk area along S 7 th (where it overlays with the drive aisle) to make pedestrians more aware that they are crossing a drive aisle. The one-way nature of the traffic flow will assist in limiting conflict points; however additional safety precautions are advised. Such delineation may also slow motorists as they enter the site and navigate turns around the building.
		+/-	
BP7	Develop alley facing facades at least one bay into the alley to eliminate harsh contrasts in the street wall. Provide adequate lighting at alleys to enhance visibility and safety. Design alley access points for vehicles with appropriate lighting and signage for quick orientation by motorists and safety for pedestrians.		The motel is developed one bay into Smilin' Irishman Alley. At a later phase, the hotel will also be built along the alley street wall. The hotel building design is not being reviewed under this application. See conditions of approval.
		+	
BP8	Exterior lighting should be designed to be visually integrated into the exterior design of the building. Lighting should be designed to provide illumination that creates a greater sense of activity, security and interest to the pedestrian.		No lighting details were provided by the time of this report. See conditions of approval.
		TBD	

5 Parking, Vehicular Use and Access

Checklist

Parking garages, surface parking, and vehicular use areas should have the same qualities and characteristics as any other downtown developments. They should relate strongly to their context, reinforce the urban street wall, and be designed to promote comfort and safety for pedestrians.

- | | | | |
|-----|---|-----|--|
| + | Meets Guidelines | NA | Not applicable |
| - | Does not meet Guidelines | TBD | To be determined; insufficient Information |
| +/- | Meets Guidelines with conditions as noted | | |

	Guideline	Finding	Comment
P1	<i>Parking garages.</i> A parking garage visible from the street should be integrated into its surroundings and provide active and inviting street level appearance. The garage should: 1) Follow all principles and guidelines for building and site design; 2) Minimize the use of ramped floors visible from the street; 3) Openings and entrances should be in scale with people. Parking garage entries shall be minimized in size but fully articulated as an opening in the structure. Pedestrian entrances should also be fully articulated; 4) Screen parked cars from pedestrian view.		
		NA	No parking garage is proposed.
P2	<i>Surface parking lots and vehicle use areas.</i> Generally speaking, development of downtown sites solely as surface parking lots or vehicle use areas is discouraged due to their negative impact on the street wall, streetscape, and pedestrian-friendly character of downtown. Specifically, demolition of buildings for development of new surface parking lots is discouraged. Surface parking and vehicle use areas elsewhere, when deemed appropriate, should adhere to the following: 1) Surface parking and vehicle use areas should not create gaps along the street and sidewalk. They should be fully screened from pedestrian view through a combination of solid building-like elements such as colonnades, decorative fencing, and dense decorative landscaping intended to continue the street wall. Dense landscaping intended for screening should		
		+	The current subject site only has one existing building, which is surrounded by surface parking. The new surface parking lot will be internal to the site and almost completely screened by the motel buildings. Interior landscaping and trees will be provided internally to the site. The surface lot and dumpster are located behind the motel building. If built, the hotel will also help with screening from the west alley in the later construction phase.

	be three feet high at time of planting and maintained to properly screen vehicular uses yet be low enough to maintain visibility and safety. The screening may also be an opportunity for public art; 2) Provide adequate interior landscaping, especially shade trees; 3) When associated with a principal structure on the same site, surface parking, loading, and waste/recycling storage and collection areas shall be located fully behind the principal structure on the site.		
P3	Vehicular access and design: 1) Curb cuts and vehicular access shall be designed in conformance with the Access Management Standards and Design Manual within the Land Development Code and appropriate Metro agencies; 2) Vehicular access should be designed to minimize conflicts between cars and pedestrians; 3) Access from the alley shall be utilized to the fullest extent possible. Where the alley is unimproved or of insufficient width or length for the new development it shall be improved as part of the project for viable use; 4) Existing curb cuts that are not proposed to be re-used should be removed and replaced with walk and curb compatible with the current standard design for that location; 5) Existing curb cuts to be re-used should be minimized in width and number to the fullest extent possible; 6) Driveways should be located to be shared with adjacent properties whenever possible; 7) Driveways and vehicular entrances should not occur in dominant locations on the site; 8) Provide adequate directional information for motorists.	+	There are currently 5 curb cuts along S 7 th and 1 along York accessing the subject site. Under the new proposal, there will be no curb cuts along York and only one to the north along S 7 th , greatly reducing the potential conflicts between pedestrians and vehicles. One access point will also be located at the opposite southwest corner from Smilin' Irishman Alley.

6 Open Space

Checklist

New public open space, exterior and interior, should be located and designed to relate strongly to pedestrians and its built context.

Design public open spaces to promote a visually pleasing, safe, and active environment for workers, residents, and visitors. Views and solar access from the principal area of the open space should be especially emphasized.

+ Meets Guidelines

NA Not applicable

- Does not meet Guidelines

TBD To be determined; insufficient Information

+/- Meets Guidelines with conditions as noted

Guideline	Finding	Comment
O1 Generally open spaces should be oriented to receive maximum sunlight and taking advantage of views to the surrounding context.	+	The proposed open space is located at the northwest corner of the site, next to a surface parking lot, and will receive maximum sunlight. However, it is unclear if this space is proposed to be public or private.
O2 To maximize the impact of the open space without degrading the street wall character of downtown exterior open spaces should: 1) Be located 100 feet away from other open spaces and intersections; 2) Be no wider or deeper than 100 feet.	+	While exact dimensions of the open space have not been provided, it is not located within 100 feet of other open space or a street intersection, and does not appear to be over 100 feet in depth or width.
O3 To create the best environment for public enjoyment new open spaces should: 1) Create a comfortable and inviting place to rest; 2) Provide clear visual and physical access from the public way; 3) Provide plenty of seating (about one linear foot for every 30 square feet of paved open space); 4) Provide sufficient lighting for safe evening use; 5) Utilize fountains or other water features; 6) Use plant materials that are colorful, appropriately sized, and attractive year round; 7) Incorporate public art in accordance with Principle 9.	TBD	Details concerning the open space, other than the planting of 6 trees, have not been provided at the time of this report. Due to its internal location, staff recommends this space be programmed for either public or internal, private guest use only if not supervised. See conditions of approval.

7 Street and Sidewalk Character

Checklist

Downtown streets and sidewalks should be safe and attractive for both cars and pedestrians. Getting from one place to another should be a pleasant, comfortable, and rewarding downtown experience.

+ Meets Guidelines

NA Not applicable

- Does not meet Guidelines

TBD To be determined; insufficient Information

+/- Meets Guidelines with conditions as noted

Guideline	Finding	Comment
SS1 Developments involving new construction and or modifications to existing curb and sidewalk along the frontage(s) of the site should include improvements to the streetscape including sidewalk and curb in accordance with the current standards for that street and the Street and Road Side Design Standards of the Land Development Code.	+	Four curb cuts will be removed, and the existing sidewalks will be restored.
SS2 Street furnishings. Street furnishings including but not limited to benches, news racks, bicycle racks, and trash containers shall conform to the standards established by the Downtown Streetscape Manual.	+	Metal benches and trash receptacles are proposed along S 7 th .
SS3 Lighting. Street lighting for public streets shall conform to the standards established by the Downtown Streetscape Manual. On-site lighting should also be compatible with street lighting standards.	TBD	Lighting details were not provided at the time of this report. See conditions of approval.
SS4 For the sake of visual continuity, trees shall be planted in the right-of-way every 25 feet to 35 feet along the curb line in order to create a continuous canopy. Tree species and caliper and planting area shall conform to the standards established by the Downtown Streetscape Manual. A mix of ornamental and shade trees should be planted outside the right-of-way for both shade and visual variety.	+	4 trees are located along the street but are located on the internal sidewalk edge of the subject property – 2 along York and 2 along S 7 th . 4 tree wells and street trees will be located along York, and 6 along S 7 th .
SS5 Pedways. The pedestrian environment is substantially impacted by the actual number of pedestrians engaging in activity on the street level. Generally speaking, the more pedestrians engaging in activity at street level the more the streetscape environment feels active, inviting and safe. Overhead pedestrian walkways (pedways) have real potential to negatively impact the street level environment by removing pedestrians from it. Overhead pedestrian structures therefore are generally discouraged. When a pedway is deemed to be appropriate, it should: 1) Be designed to be architecturally compatible with the design of the structures at each end; 2) Have articulated street level access or clear direction to access points for pedestrians; 3) Provide entry points within each building that are comparable to street level entrances like lobbies that are attractive and have clear directions to internal circulation elements and other internal uses; 4) Give consideration for views underneath including material finish and lighting; 5) Not have permanent signage.	NA	

(a) These guidelines are in addition to the streetscape guidelines of the Department of Public Works and Assets, the Metro Arborist, and the Land Development Code.

(b) Project features developed as public amenities should also be designed in conformance with Metro Louisville Complete Streets Manual, October 2007, which promotes the design of transportation corridors that are safe and convenient for pedestrians, cyclists, motorists and bus riders of all ages and abilities.

9 Public Art

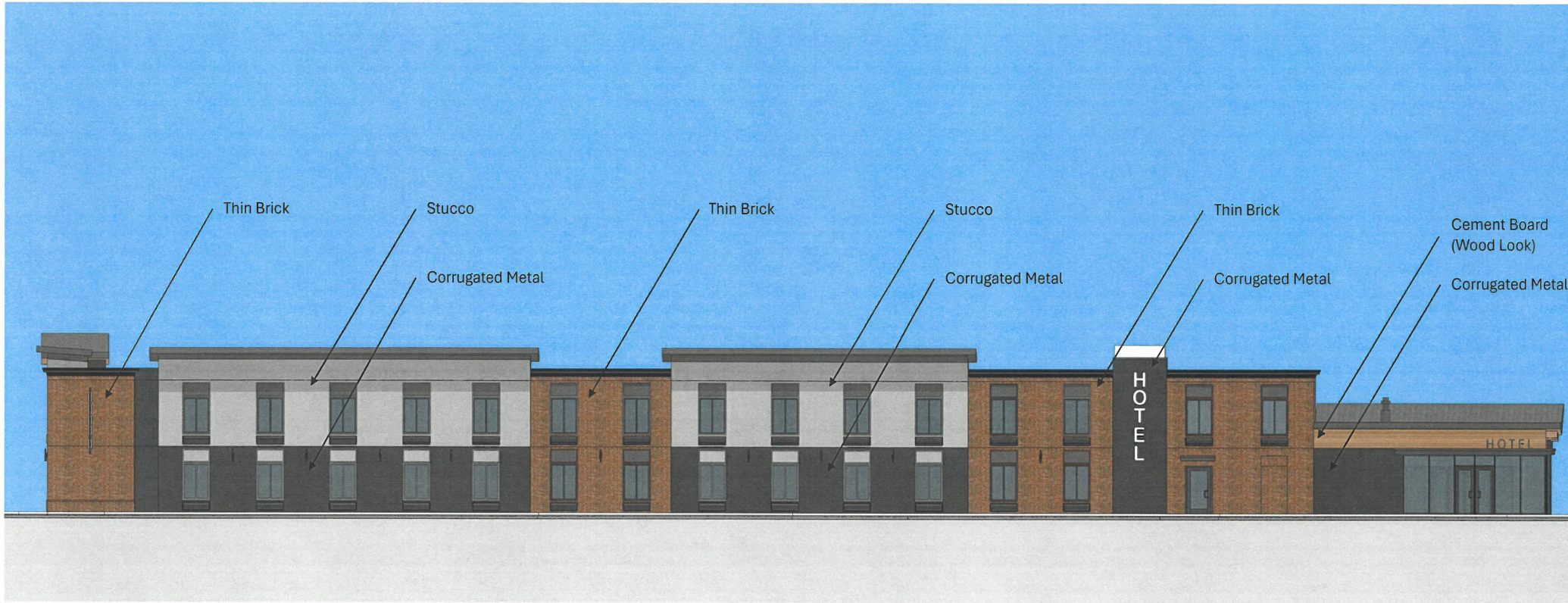
Checklist

Public art is the continuously growing record of the community's identity, creativity, values, and its highest aspirations. Physically, public art is an element or installation of art - temporary or permanent - placed on public or private property including building exteriors and outdoor public areas for enjoyment by all. Incorporation of public art elements in downtown development is highly encouraged.

- + Meets Guidelines NA Not applicable
- Does not meet Guidelines TBD To be determined; insufficient Information
- +/- Meets Guidelines with conditions as noted

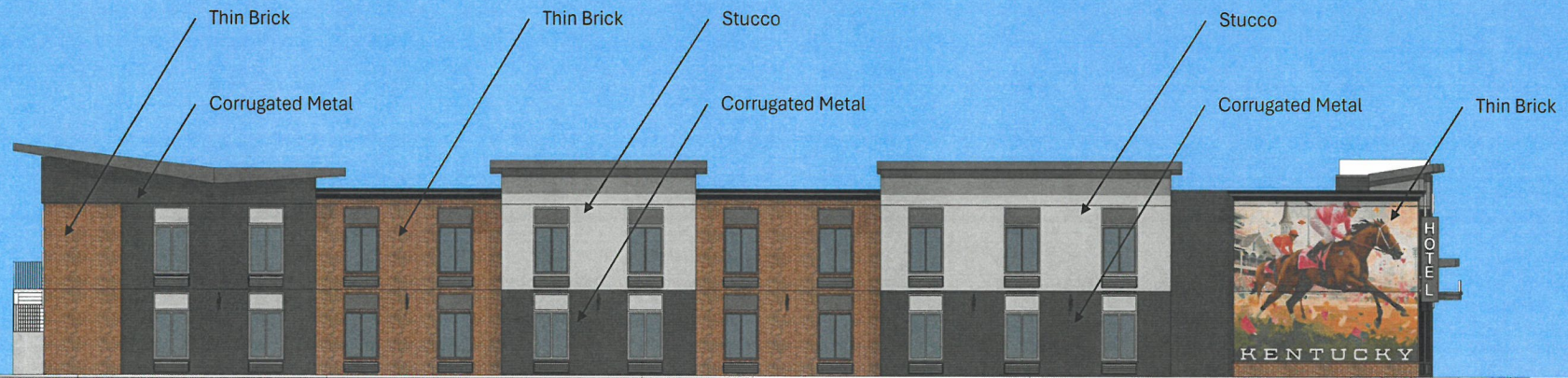
	Guideline	Finding	Comment
PA1	Public art should be available for the enjoyment and enrichment of all the people within the community. Inclusion of a meaningful allowance for the commissioning of public art in the planning and construction for all significant building projects is encouraged.	+	A location on a wall facing York Street has been shown for public art. The specific design has not been provided at this time. See conditions of approval.
PA2	The public art planning and selection process should begin at the onset of individual projects. It should be designed, executed to integrate the artwork with the overall project and aesthetically enhance the urban environment.	TBD	The specific design has not been provided at this time. See conditions of approval.
PA3	Public art proposals should conform to the design guidelines and master plan established by the Mayor's Committee for Public Art (MCOPA). The MCOPA guidelines and master plan recognize public art's potential to: 1) Demonstrate Louisville's values of community and creativity; 2) Stimulate economic growth and investment; 3) Encourage public education by improving cultural literacy; 4) Contribute to Louisville's identity; 5) Bridge cultural and social barriers; 6) Provide community cohesion; 7) Energize public spaces in the urban environment; 8) Enhance new and existing development; 9) Provide local opportunities for talented regional, national, and international artists; 10) Contribute to Louisville as a tourist destination; 11) Leave a legacy for the future.	TBD	The specific design has not been provided at this time. See conditions of approval.

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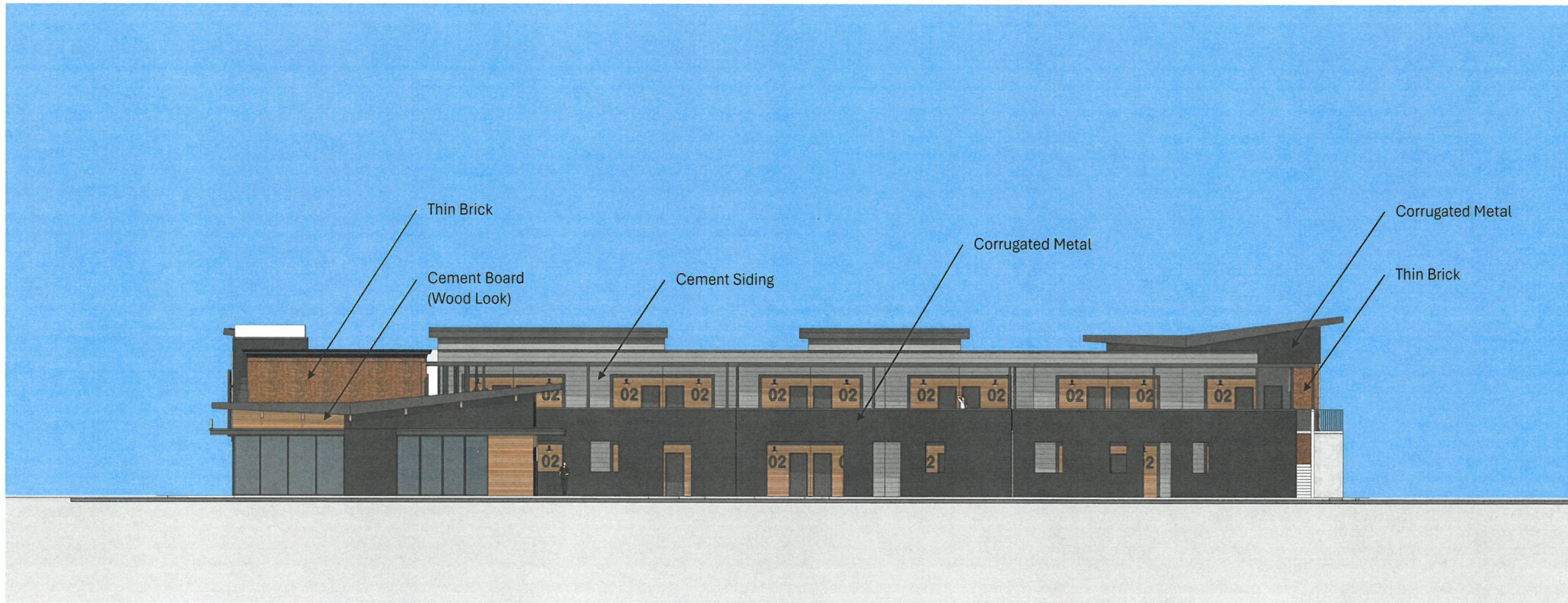
East Elevation NTS

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South Elevation NTS

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North Elevation NTS



TGC GROUP

TGC HOTEL – Louisville, KY

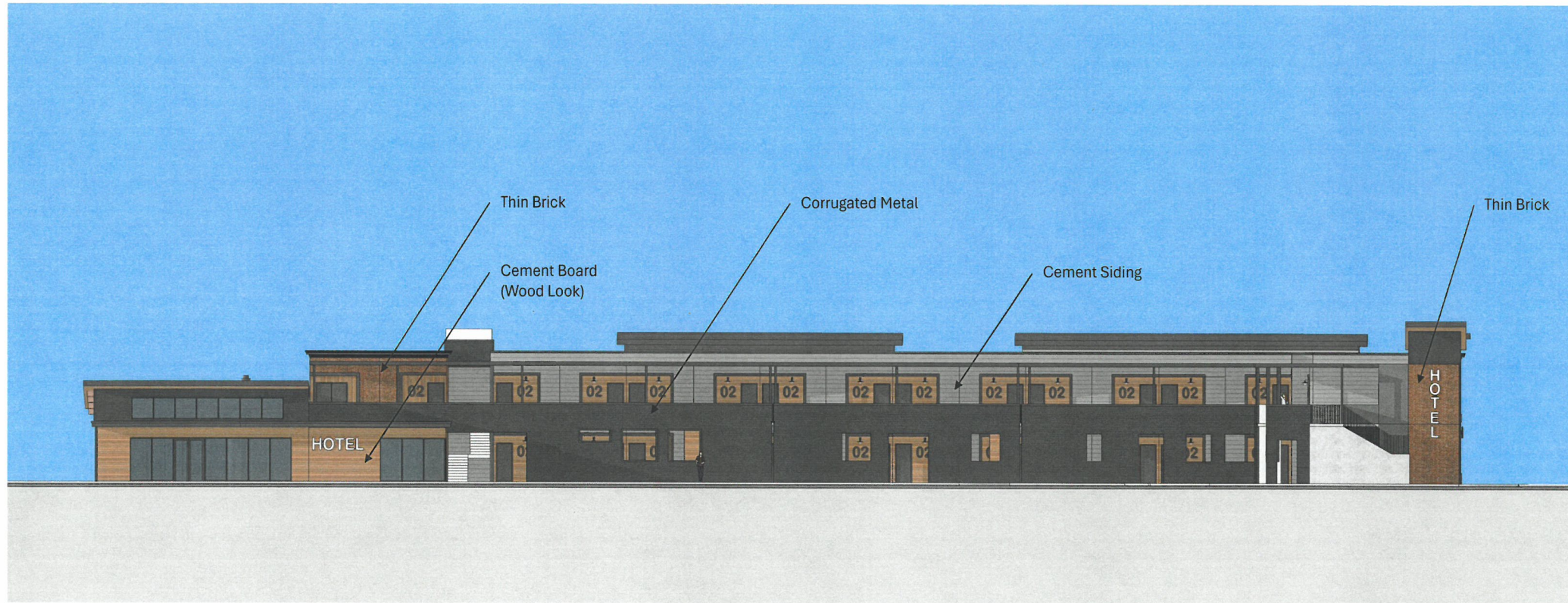
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SHELDEN
ARCHITECTURE

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West Elevation NTS



TGC GROUP

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Northeast Corner - Entr

SHULDEN
ARCHITECTURE

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Northeast Corner



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Southeast Corner



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SITE PLAN-FIRST FLOOR
1" = 10'-0"

LOUISVILLE HOTEL

720 & 726 S 7TH ST LOUISVILLE, KY

SD LEV 1

SHULDEN
ARCHITECTURE

06 MAY 24

24-overlay - 0016



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SITE PLAN-SECOND FLOOR

LOUISVILLE HOTEL

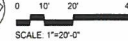
720 & 726 S 7TH ST. LOUISVILLE, KY

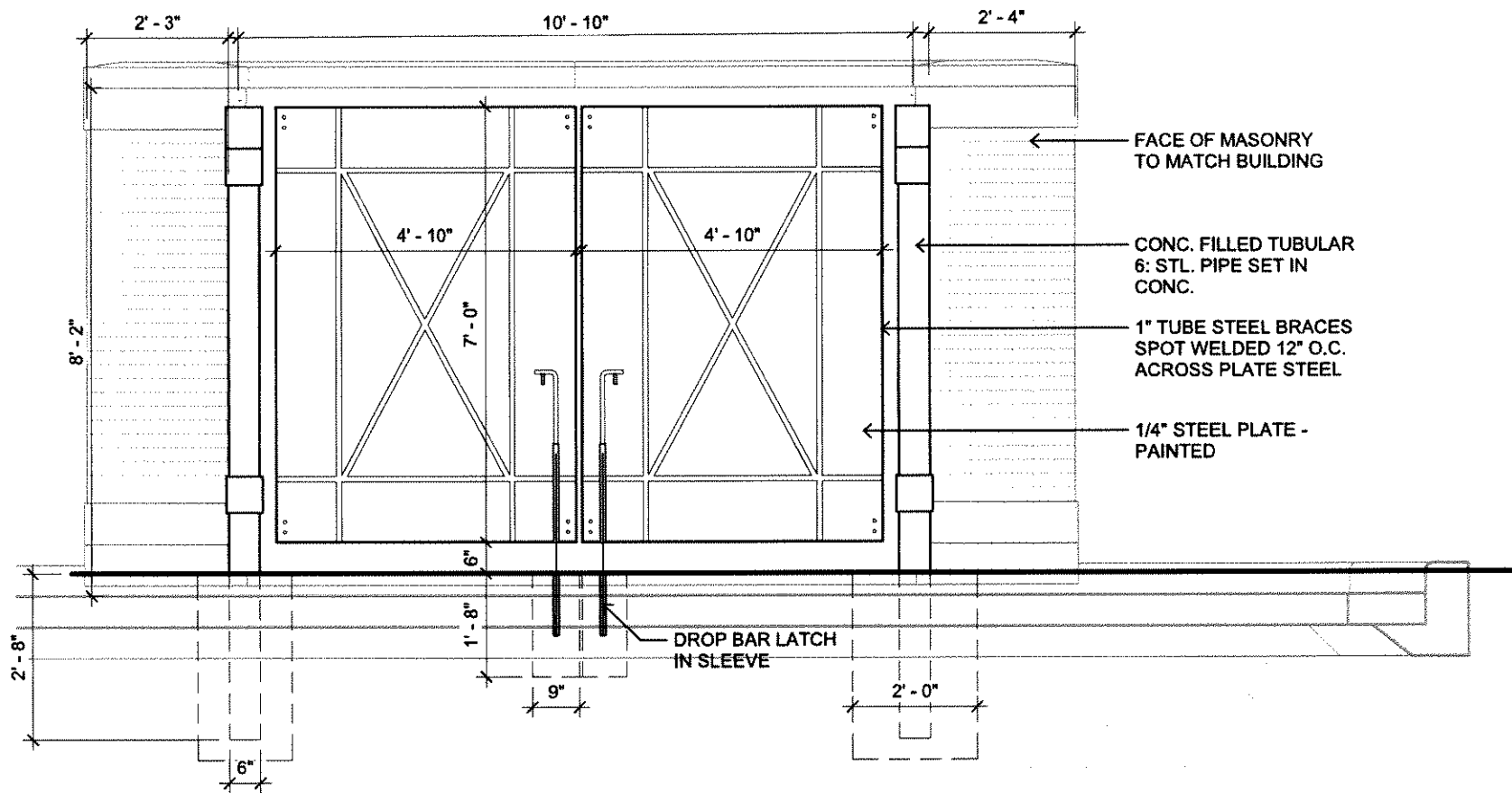
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SHELDEN
ARCHITECTURE

06 MAY 2024

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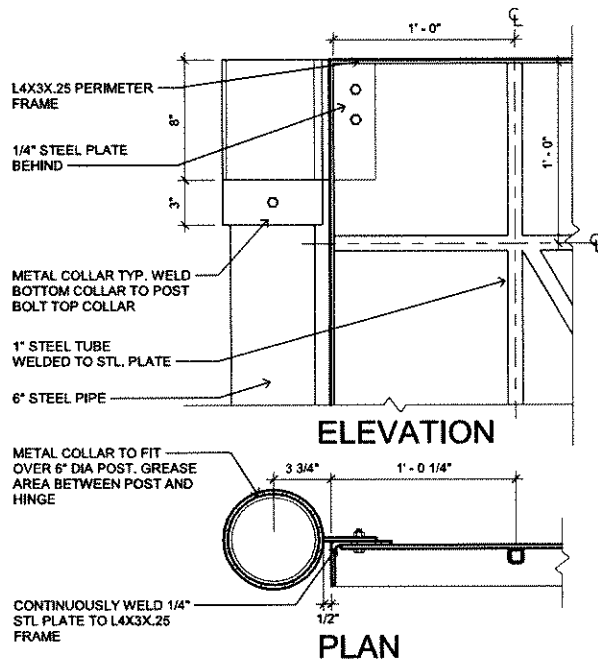




8 ENCLOSURE WALL

0 1'-0" 2'-0" 4'-0"

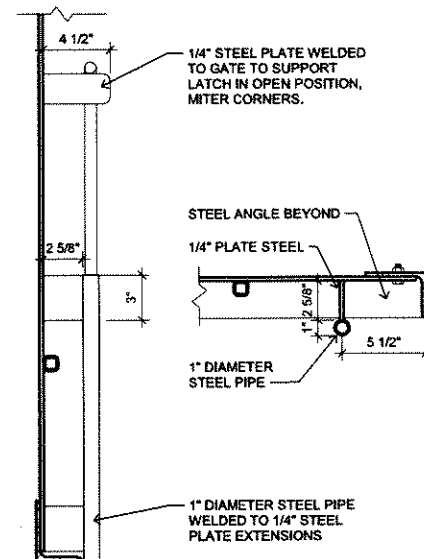
3/8" = 1'-0"



3 GATE HINGE DETAIL

0 3" 6" 1'-0"

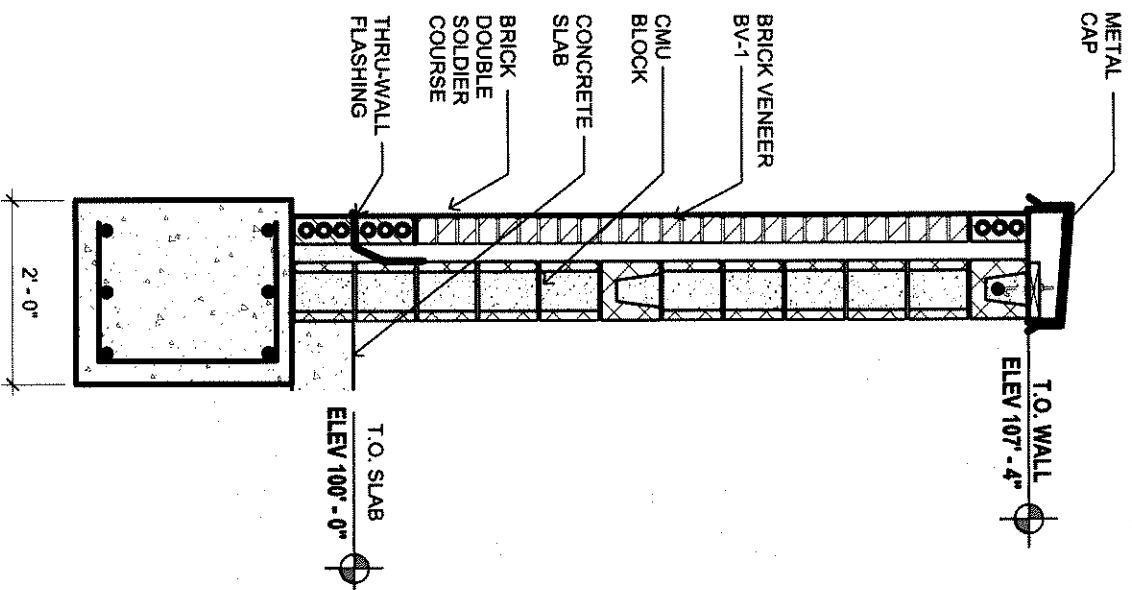
1 1/2" = 1'-0"



4 GATE LATCH DETAIL

0 3" 6" 1'-0"

1 1/2" = 1'-0"



5 SECTION

0 1'-0" 2'-0" 4'-0"

1/2" = 1'-0"



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24-overlay-0016



Distilled
Spirits
Epicenter

Global
Restaurant

Imanka Somali
Restaurant

GQ Unlimited

Pediatric &
Neonatal
Specialists

Dr. Stephanie
H. Dailey, MD

White Castle

American Nails

U.S. Attorney's Office
for the Western
District of Kentucky

Gene Snyder

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Danny's
Transmission
Service

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MSD NOTES

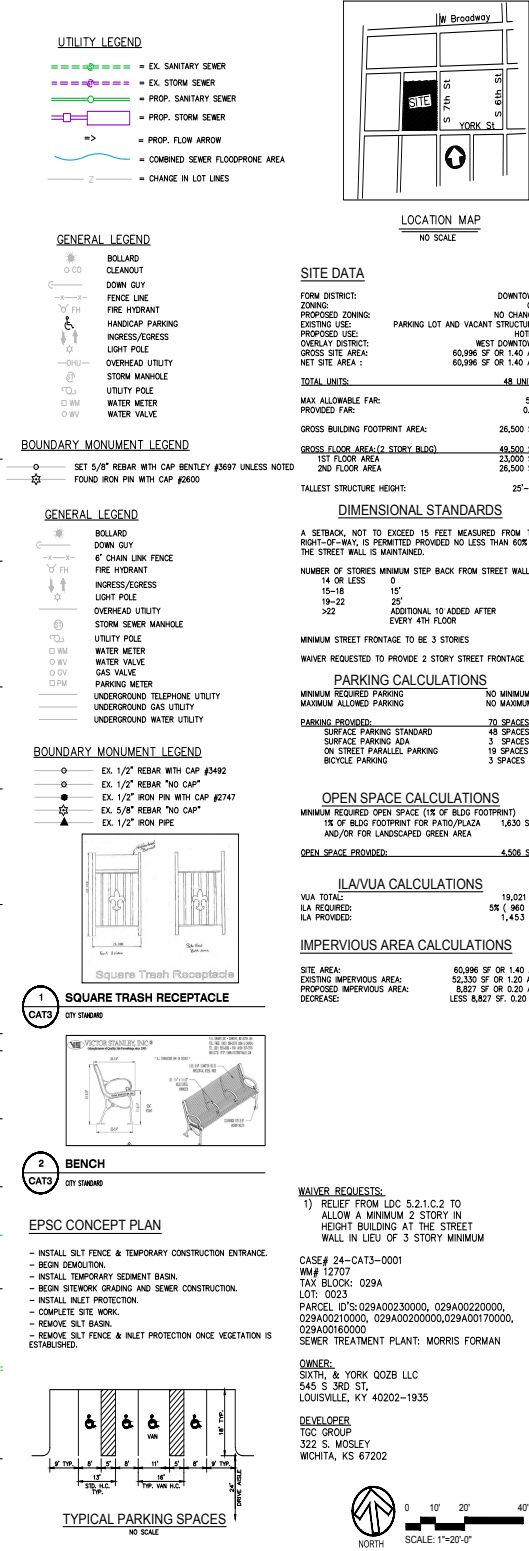
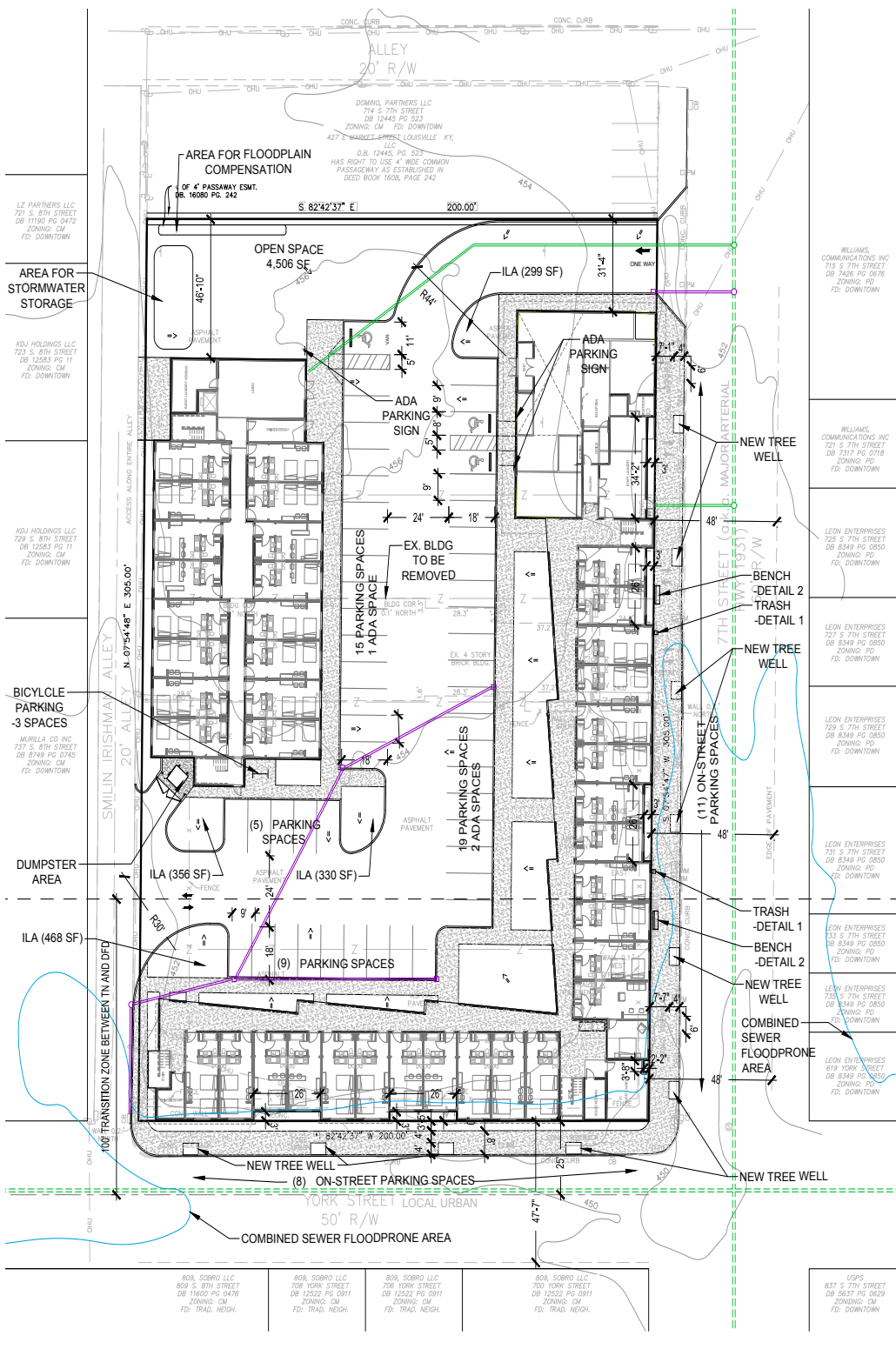
- CONSTRUCTION PLANS AND DOCUMENTS SHALL COMPLY WITH LOUISVILLE AND JEFFERSON COUNTY METROPOLITAN SEWER DISTRICT'S DESIGN MANUAL AND STANDARD SPECIFICATIONS AND OTHER LOCAL, STATE AND FEDERAL ORDINANCES.
- THE PROPOSED DRAINAGE AND STORM SEWERS SHOWN ON THE PLAN ARE CONCEPTUAL. FINAL DESIGN SHALL BE DETERMINED DURING THE CONSTRUCTION PLAN DESIGN PROCESS AND SHALL CONFORM TO MSD REQUIREMENTS.
- THIS PROPERTY IS NOT LOCATED IN A 100 YEAR FLOOD HAZARD AREA. (FEMA MAP 21110041F DECEMBER 5, 2006)
- SANITARY SEWERS ARE AVAILABLE BY CONNECTION. SANITARY SEWER PATTERN DERIVED IS FOR CONCEPTUAL PURPOSES ONLY. FINAL CONFIGURATION AND SIZE OF SEWER PIPES SHALL BE DETERMINED DURING THE CONSTRUCTION PLAN DESIGN PROCESS. SANITARY SEWER FACILITIES SHALL CONFORM TO MSD REQUIREMENTS.
- DRAINAGE / STORM WATER DETENTION. FINAL CONFIGURATION AND SIZE OF DRAINAGE PIPES AND CHANNELS SHALL BE DETERMINED DURING THE CONSTRUCTION PLAN DESIGN PROCESS. DRAINAGE FACILITIES SHALL CONFORM TO MSD REQUIREMENTS.

- TOTAL EXCESS RUNOFF CALCULATION
THERE IS MORE PREVIOUS PROPOSED THAN CURRENTLY EXISTS THEREFORE THERE WILL BE NO ADDITIONAL RUNOFF
- SITE DISCHARGES INTO THE COMBINED SANITARY SEWER SYSTEM AND SHALL LIMIT THE 100-YEAR POST-DEVELOPED DISCHARGE TO THE 10-YEAR PRE-DEVELOPED DISCHARGE PER SECTION 10.3.1.2 OF THE MSD REGIONAL MANUAL. SITE IS A TOTAL NET REDUCTION OF IMPERVIOUS AREA BUT SOME ADDITIONAL STORM WATER MITIGATION MAY BE REQUIRED ONCE THE CONSTRUCTION PLANS ARE EVALUATED.
 - EROSION & SILT CONTROL. A SOIL AND SEDIMENTATION CONTROL PLAN SHALL BE DEVELOPED AND IMPLEMENTED IN ACCORDANCE WITH MSD AND THE USDA NATURAL RESOURCES CONSERVATION SERVICE RECOMMENDATIONS. DOCUMENTATION OF MSD'S APPROVAL OF THE PLAN SHALL BE SUBMITTED TO THE PLANNING COMMISSION PRIOR TO GRADING AND CONSTRUCTION ACTIVITIES.
 - THE FINAL DESIGN OF THIS PROJECT MUST MEET ALL MSD WATER QUALITY REGULATIONS ESTABLISHED BY MSD. SITE LAYOUT MAY CHANGE AT THE DESIGN PHASE DUE TO PROPER SIZING OF GREEN BEST MGMT. PRACTICES.
 - MSD DRAINAGE BOND REQUIRED PRIOR TO CONSTRUCTION PLAN APPROVAL.
 - ANY REQUIRED FILL IN THE COMBINED SEWER OVERFLOW FLOODPLAIN SHALL BE COMPENSATED ON SITE AT 1.5 TO 1.
 - A PORTION OF THIS SITE IS LOCATED WITHIN THE COMBINED SEWER OVERFLOW FLOODPLAIN. LOWEST FINISHED FLOOR TO BE AT OR ABOVE 452.1 AND LOWEST MACHINERY TO BE AT OR ABOVE 453.1.

- ## GENERAL NOTES
- ALL HANDICAP SITE FEATURES SHALL BE CONSTRUCTED IN ACCORDANCE WITH ACCEPTABLE "ADA" REQUIREMENTS FOR HANDICAP ACCESSIBILITY.
 - ALL CONSTRUCTION AND SALES TRAILERS MUST BE PERMITTED BY THE DEPARTMENT OF PUBLIC HEALTH AND WELFARE IN ACCORDANCE WITH CHAPTER 115 OF LOUISVILLE JEFFERSON COUNTY METRO ORDINANCES.
 - MOSQUITO CONTROL SHALL BE PROVIDED IN ACCORDANCE WITH CHAPTER 96 OF LOUISVILLE JEFFERSON COUNTY METRO ORDINANCES.
 - CONSTRUCTION PLANS, BOND & PERMIT REQUIRED FOR ALL WORK DONE WITHIN THE RIGHT-OF-WAY.
 - MITIGATION MEASURES FOR DUST CONTROL SHALL BE IN PLACE DURING CONSTRUCTION TO PREVENT FUGITIVE PARTICULATE EMISSIONS FROM REACHING EXISTING ROADS AND NEIGHBORING PROPERTIES.
 - THE DEVELOPMENT LIES IN THE LOUISVILLE #2 FIRE DISTRICT.
 - SEWERAGE AND ROADWAY PAYMENT METHODS WILL BE PROVIDED AS REQUIRED IN CHAPTER 6 OF THE LAND DEVELOPMENT CODE.
 - COMPATIBLE UTILITY LINES (ELECTRIC, PHONE, CABLE) SHALL BE PLACED IN A COMMON TRENCH UNLESS OTHERWISE REQUIRED BY APPROPRIATE AGENCIES.
 - ALL SERVICE STRUCTURES SHALL BE SCREENED IN ACCORDANCE WITH CHAPTER 10 OF THE LAND DEVELOPMENT CODE. SERVICE STRUCTURES INCLUDING BUT ARE NOT LIMITED TO: PROPANE TANKS, DUMPSTERS, HVAC UNITS, ELECTRIC TRANSFORMERS AND TELECOM BODIES.
 - EXISTING SIDEWALK RECONSTRUCTION AND REPAIRS SHALL BE REQUIRED, AS NECESSARY, TO MEET CURRENT METRO PUBLIC WORKS STANDARDS AND SHALL BE INSPECTED PRIOR TO FINAL BOND RELEASE.
 - ALL EXISTING BUILDINGS ON THE SUBJECT SITE TO BE REMOVED.
 - EXISTING CURB CUTS TO BE REMOVED AND CURB WILL BE REESTABLISHED IN THE RIGHT OF WAY.
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- ## EPSC NOTES
- THE APPROVED EROSION PREVENTION AND SEDIMENT CONTROL (EPSC) PLAN SHALL BE IMPLEMENTED PRIOR TO ANY LAND-GRADING ACTIVITY ON THE CONSTRUCTION SITE. ANY MODIFICATIONS TO THE APPROVED EPSC PLAN MUST BE REVIEWED AND APPROVED BY MSD'S PRIVATE DEVELOPMENT REVIEW OFFICE. EPSC BMP'S SHALL BE INSTALLED PER THE PLAN AND MSD STANDARDS.
- DETENTION BASINS, IF APPLICABLE, SHALL BE CONSTRUCTED FIRST AND SHALL FUNCTION AS SEDIMENT BASINS DURING CONSTRUCTION UNTIL THE CONTRIBUTING DRAINAGE AREAS ARE SEEDS AND STABILIZED.
- ACTIONS MUST BE TAKEN TO MINIMIZE THE TRACKING OF MUD AND SOIL FROM CONSTRUCTION AREAS ONTO PUBLIC ROADWAYS. SOILS TRACKED ONTO THE ROADWAY SHALL BE REMOVED DAILY.
- SOIL STOCKPILES SHALL BE LOCATED AWAY FROM STREAMS, PONDS, AND ADJACENT CATCH BASINS. STOCKPILES SHALL BE SEEDS, MOULDED, AND SUCCESSIONALLY CONTAINED THROUGHOUT THE USE OF SILT FENCING.
- ALL STREAM CROSSINGS MUST UTILIZE LOW-WATER CROSSING STRUCTURES PER MSD STANDARD DRAWING DR-02.

- WHERE CONSTRUCTION OR LAND DISTURBANCE ACTIVITY WILL BE TEMPORARILY LOCATED ON ANY PORTION OF A SITE, TEMPORARY SITE STABILIZATION MEASURES SHALL BE REQUIRED AS SOON AS PRACTICABLE, BUT NOT LATER THAN 14 CALENDAR DAYS AFTER THE ACTIVITY HAS CEASED.
- SEGMENT-LADEN GROUNDWATER ENCOUNTERED DURING TRENCHING, BORING, OR OTHER EXCAVATION ACTIVITIES SHALL BE PUMPED TO A SEDIMENT TRAPPING SERVICE PRIOR TO BEING DISCHARGED INTO A STREAM, POND, SLOVE, OR CATCH BASIN.



SABAK, WILSON & LINGO, INC.
 ENGINEERS, LANDSCAPE ARCHITECTS & PLANNERS
 608 S. THIRD STREET, LOUISVILLE, KENTUCKY 40202
 (502) 584 - 6271

SHEET TITLE: CATEGORY 3 DEVELOPMENT PLAN
 PROJECT TITLE: 726 S. 7TH STREET, KY 40203
 DEVELOPER: LDG DEVELOPMENT, LLC

JOB NO. 3293
 SCALE: 1"=30'
 DATE: 05/28/24
 DRAWING NO. CAT3